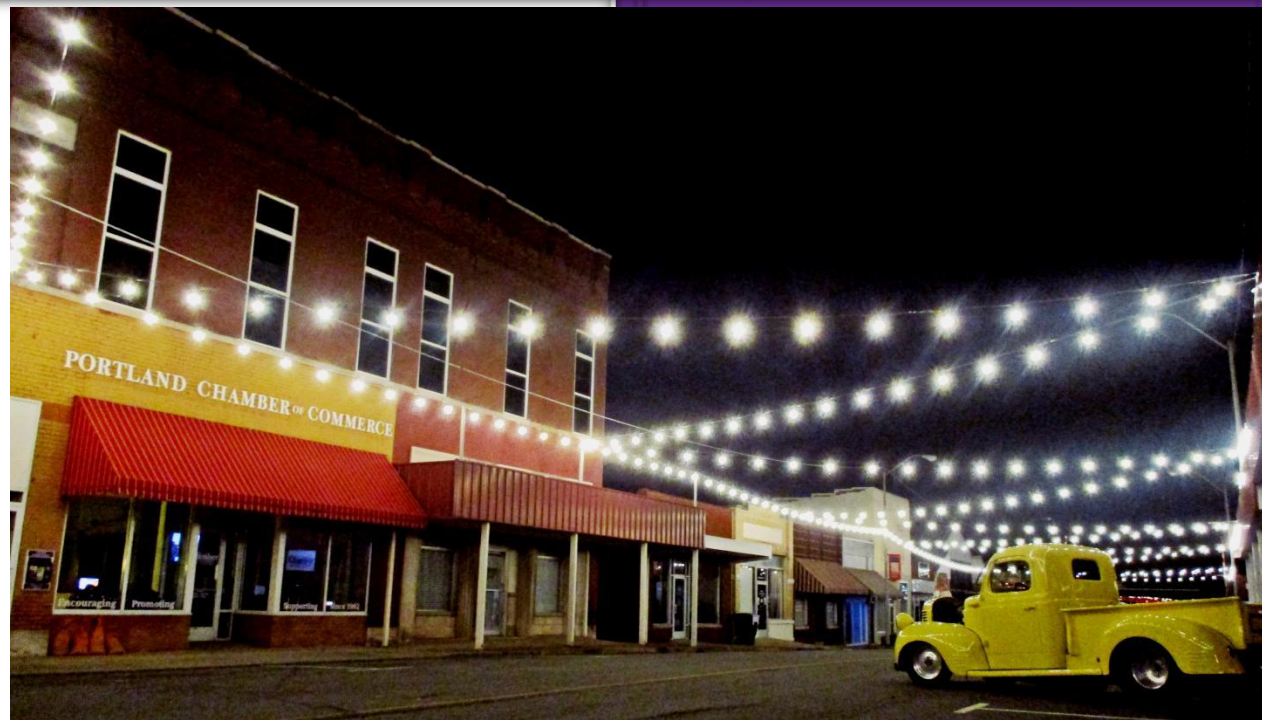


# City of Portland Design Standards



City of Portland, Tennessee  
Adopted September 3, 2019  
As Amended January 6, 2020

TABLE OF CONTENTS

List of Tables ..... iii

List of Figures ..... iii

Chapter 1 Intent & Purpose..... 1

    1.1 Intent..... 1

    1.2 Purpose ..... 2

Chapter 2 Applicability ..... 4

    2.1 Applicability of Other Articles..... 4

    2.2 Overlay or Special Districts ..... 5

Chapter 3 Site Design ..... 6

    3.1 Intent..... 6

    3.2 Vehicular and Pedestrian Circulation..... 7

        3.2.1 Pedestrian Circulation ..... 8

    3.3 Parking Areas ..... 9

    3.4 Service Areas..... 10

    3.5 Outdoor Storage ..... 11

    3.6 Roof Mounted Equipment, Dumpster Enclosures, and Ground Mounted Equipment..... 11

    3.7 Drive-Through Facilities ..... 12

    3.8 Outdoor Seating and Dining Areas ..... 13

    3.9 Fences, Walls, and Hedges..... 14

    3.10 Sight Distance Requirements..... 15

    3.11 Stormwater Management..... 15

    3.12 Special Flood Hazard Areas ..... 16

Chapter 4 Architecture ..... 17

4.1 Intent..... 17

4.2 General Architecture Design Standards ..... 18

4.3 Façade Design ..... 18

4.3.1 Façade Elements ..... 18

4.3.2 Rear and Side Facades ..... 24

4.3.3 Offsets ..... 25

4.3.4 Other Façade Elements ..... 26

4.4 Building Materials..... 26

4.5 Rooflines ..... 29

4.6 Awning and Canopies ..... 30

4.7 Alterations and Additions..... 30

Chapter 5 Lighting ..... 31

5.1 Intent..... 31

5.2 Lighting Design Standards ..... 31

Chapter 6 Procedure, Appeals, and Penalties ..... 33

6.1 Procedure..... 33

6.2 Design Standard Reviews..... 33

6.3 Appeals..... 33

6.4 Penalties ..... 34

**LIST OF TABLES**

|  |    |
|--|----|
| Table 1: Required Stacking Spaces for Drive Throughs .....               | 13 |
| Table 2: Example of Facade Elements.....                                 | 19 |
| Table 3: List of Permitted Building Materials by Zoning and/or Use ..... | 27 |

**LIST OF FIGURES**

|   |    |
|---|----|
| Figure 1: Visibility at Intersections.....              | 16 |
| Figure 2: Side Facades on a Corner Lot.....             | 24 |
| Figure 3: Building Offset Diagram .....                 | 25 |
| Figure 4: Primary and Secondary Materials Diagram ..... | 28 |
| Figure 5: Material Change Diagram .....                 | 29 |

## CHAPTER 1 INTENT & PURPOSE

---

### 1.1 INTENT

The design standards set forth in Article IV of the Combined Zoning Ordinance of City of Portland, Tennessee and the Portland Planning Region, are referred to collectively as the “Design Standards” or the “Standards.” The Standards provide a framework for citizens and developers to work toward achieving a better built environment through the attention given to fundamental design principles. The Standards are intended to affect how new development and redevelopment can contribute positively to Portland’s rural character. Design guidelines allow new development and redevelopment to respond better to the distinctive character of its surroundings.

The City of Portland’s environment is an important asset in seeking to attract residents, businesses and employment opportunities. The City seeks to preserve its quality of life and recognize its heritage and history while planning for its future. The City recognizes that the physical appearance impacts its economic prosperity. These standards communicate a set of design standards for new development and redevelopment of existing properties in the City that are aimed at ensuring that the City of Portland maintains its citizens quality of life and expectations. The goal is to ensure, over time, the standards will improve the character and natural beauty of the community making it an even more attractive and prosperous City.

The standards have are intended to:

1. To promote qualities in the environment that sustain the community’s economic well-being in order to attract diverse housing, business and community activities that provide and enhance the quality of life;
2. To foster the community’s attractiveness and functional utility as a place to live and work;
3. To preserve and protect the community’s heritage and history;
4. To insure compatibility with existing neighborhoods and business community;
5. To improve the aesthetic appearance of development throughout the City;
6. To safeguard public investment in the community; and

7. To establish a level of community expectations; and
8. To guide the expansion and renovation of existing structures and the construction of new buildings and parking within the City; and
9. To assist the City in reviewing development proposals.

## **1.2 PURPOSE**

The overriding objective of the design standards is to ensure that new development and alterations and additions fit in well with its surroundings. The following design standards share this objective and with an emphasis on siting and design conditions and priorities supported by the community, aim to guide the design of new development in a manner that strengthens the mixed-use thoroughfares in Portland. The community desires to maintain the small-town atmosphere and qualities that have historically characterized the City of Portland. However, its City officials and Citizens recognize that new development provides the opportunity for a broader mix of businesses and services, residential units, employment and an expanded tax base in order to economically thrive. These design standards will assist in ensuring that new development enhances the rural qualities valued by its citizens and creates a pattern of development that is pedestrian friendly and human scale, character and function.

These guidelines explain the goals and standards which the Portland Municipal-Regional Planning Commission (hereafter referred to as 'Planning Commission,'), Design Review Committee, and City Planning Staff will apply in reviewing development proposals. It does not however, reproduce all of the specific requirements stated in the Zoning Ordinance, Subdivision Regulations, or other applicable development standards and regulations. Applicants are advised to consult all such documents prior to preparing plans.

Additional site requirements may be found in the following documents:

1. Zoning Ordinance
  - a. Parking – Article IV, Chapter 1
  - b. Sign Regulations – Article IV, Chapter 2
  - c. Access Management – Article IV, Chapter 3

- d. Landscaping, Screening, and Buffering – Article IV, Chapter 4
- e. Commercial – Article VII
- f. Industrial – Article VIII
- g. Floodplain Management – Article IX
- h. Airport Overlay District – Article XI
- i. Commercial PUD – Article XII, Chapter 4
- j. Industrial PUD – Article XII, Chapter 5

This list is not intended to be an all-inclusive list but rather a quick reference guide.

## CHAPTER 2 APPLICABILITY

---

The standards that follow outline these purposes into criteria for design. This article outlines the process which applicants must follow to seek approval of their development projects by the City of Portland Municipal-Regional Planning Commission and City Planning Staff.

Projects subject to these Design Standards.

1. Any non-residential development or multi-family (more than two dwelling units attached or detached) development involving the issuance of a building permit for construction or reconstruction, is subject to design review as follows:
  - a. Any addition to an existing structure totaling less than 50% of ground floor area, shall make design standard improvements to the Site in proportion to the addition proposed.
2. Any new non-residential development or multi-family development (more than two dwelling units attached or detached), which includes any buildings, structures, or physical improvements or changes to land, is subject to design review as follows:
  - a. Sites which are currently vacant and the site is considered new construction on a legal lot of record or lease of land, or;
  - b. Any addition attached to an existing structure totaling 50% or more of ground floor area, or;
  - c. Change or expansion in the off-street parking layout of an existing building on legal lot(s) of record. If the change or expansion results in a twenty-five percent (25%) or more increase in spaces then the entire parking lot shall be brought into compliance with the current regulations contained in the Combined Zoning Ordinance of the City of Portland, Tennessee and the Portland Planning Region (hereafter referred to as 'Zoning Ordinance'.)

### 2.1 APPLICABILITY OF OTHER ARTICLES

1. Unless exemptions or alternate standards are explicitly established in the Zoning Ordinance or these Standards, all standards and provisions of these guidelines shall apply to development within any zoning district.
2. If conflicts exist between the requirements of these Standards and the Zoning Ordinance, the requirements of these Standards shall supersede.



## **2.2 OVERLAY OR SPECIAL DISTRICTS**

Where a zoning district is overlaid by an Overlay or Special District, the standards set forth within that district shall also apply.

## **CHAPTER 3 SITE DESIGN**

---

This Chapter (Site Design Standards) shall apply to multi-family, mixed use, community facility, commercial, and industrial site plans. These standards shall not apply single-family detached or two-family dwelling detached units, unless three or more dwelling units are built on a single lot.

### **3.1 INTENT**

Each property in the City of Portland's industrial, commercial and mixed-use areas is unique and each needs to be developed with a careful understanding of the site to meet the needs of the proposed business while also improving the visual character, safety and function of the area.

1. Encourage development that respects the uniqueness of each property and reinforces the City of Portland's historic character and sense of place, and;
2. Creates an attractive, functional, and safe environment that is beneficial to business, and;
3. Encourages walking and cycling to, and within, the area by providing safe, interconnected development, and;
4. Includes access management to increase public safety and;
5. Protects abutting residential properties through sensitive site planning, buffering, and architectural designs and;
6. Preserves significant natural and cultural features and;
7. Is organized in such a way to create or enhance a village quality versus lineal strip development and;
8. Focuses on the visual character of Portland, including preservation of historic properties through adaptive reuse.

### **3.2 VEHICULAR AND PEDESTRIAN CIRCULATION**

All development will be characterized by safe, user-friendly and efficient traffic flow. Projects will be designed to reduce the number of curb cuts, provide for safe vehicular and pedestrian movement, interconnectivity between sites and reduce the number of roadway trips.

1. Site plans shall be designed to minimize the number of curb cuts onto city and state roads to increase vehicular and pedestrian safety. No more than one curb cut should occur in each 100 feet of frontage, unless access management standards apply.
2. Entrances to abutting commercial properties shall be combined whenever possible. If two entries are needed, a one- way system between the properties are encouraged on all collector streets, and required on all arterial streets.
3. The Site Plan shall be designed to provide pedestrian safety by separating vehicles from pedestrian areas to the greatest extent possible.
4. Where feasible, connections between parking lots and driveways on adjacent properties shall be utilized. Interconnectivity shall provide a safe, direct access between adjacent lots in a manner that prevents them from becoming a shortcut between roadways.
5. Cross access easements may be required to allow interconnectivity.
6. Safe pedestrian and bicycle connections between abutting land uses shall be provided where possible to encourage foot and bicycle traffic and minimize vehicular traffic.
7. Drives leading to and from drive-through shall minimize conflicts with pedestrian circulation and normal vehicular traffic. When there is conflict with pedestrian circulation, techniques will be used to increase motorist awareness, such as signage, lighting, raised sidewalks, changes in paving, or other devices.
8. Drive through queuing shall not be located in the required parking areas or other areas which would cause unsafe conditions or congestion.
9. For all driveways greater than 32 feet wide, a 5' foot minimum width pedestrian island shall be installed at the crosswalk for pedestrian refuge.

10. For safe pedestrian movement within the site, service and delivery drives shall be separated from internal walkways, parking areas or pedestrian use areas by landscaped islands.

### **3.2.1 Pedestrian Circulation**

Planning for safe, comfortable, and pleasant movement of pedestrians in and through sites on sidewalks is important for the health of the City of Portland. Sidewalks can add value to the city by connecting people in places where they can invest locally, share ideas, and build community. The value added by having sidewalks can be different from location to location. A context appropriate evaluation of sidewalk needs is an effective way to add value to the city.

1. All new construction and additions of more than five thousand (5,000) square feet, shall provide a Pedestrian Circulation Plan. This plan shall:
  - a. Show how pedestrians will get from the main building to:
    - i. The parking lot
    - ii. Any required through sidewalks
    - iii. The dumpster pad
    - iv. Other outbuildings and site amenities
  - b. If required in this section, show how a safe, comfortable, and pleasant way for pedestrians to travel through the property from one corner at the public right-of-way to the other corner at the public right-of-way. If a site is at an intersection, the plan shall show how pedestrians can safely move from each corner at the public right-of-way to the corner at the intersection.
  - c. The Pedestrian Circulation Plan shall show how pedestrians can safely, comfortably, and pleasantly circulate.
2. Existing sidewalks in need of repair or not in compliance with Subsection (3) shall be repaired and/or replaced in accordance with Subsection (3). Any newly constructed sidewalk shall comply with the requirements as stated in Subsection (3).
3. Sidewalks shall comply with the following requirements:
  - a. Shall be constructed in accordance with ADA requirements; and

- b. Shall be a minimum of five (5) feet in width; and
  - c. All sidewalks shall be unobstructed, meaning that while sidewalks can be wider than the minimum width, the minimum width shall not be obstructed; and
  - d. Sidewalks shall be installed five (5) feet from the back of the street curb to the nearest edge of the sidewalk. In such locations where site constraints do not permit a five (5) foot grass area, the Zoning Administrator shall have the authority to reduce the setback of the sidewalk. Such relief shall be the minimum deviation that will make possible the reasonable use of the site.
- 4. Sidewalks shall be required for all new developments except on industrial developments or in any area that the Planning Commission identifies as inappropriate for pedestrian traffic in a Bicycle and Pedestrian Plan.
  - 5. Sidewalks shall not be required in industrial zones unless there is a known safety concern that in the opinion of the Zoning Administrator would be eliminated by the construction of a sidewalk.

### **3.3 PARKING AREAS**

Parking lots shall be designed to complement the buildings on site, adjacent buildings, and physical characteristics for the site and the overall character of the City of Portland. Parking lots should not be a dominant visual element in relation to the site and surrounding properties. The scale of the parking lot shall be reduced by minimizing the amount of paved surface and parked vehicles from the road. Site plans shall be designed so the parking lots are inviting, pedestrian friendly and pay careful attention to internal walkways, landscaping and lighting while balancing the needs of the vehicles and pedestrians. Refer to Article IV, Chapter 1 of this Ordinance for general parking design standards.

- 1. The majority of the parking area shall be located at the rear or sides of the commercial buildings. Parking may be allowed in front of buildings if out parcels are provided to act as a buffer from the right-of-way.
- 2. Parking that is adjacent to residential zone lots, the lot shall be sufficiently screened with trees and shrubs. The site plan will be designed with the parking coordinated with building entrances, proper lighting and landscaping.

3. Paved surfaces of parking areas shall be separated from all buildings by a minimum of five (5) feet for a one-story building or eight (8) feet for a two or more-story building.
4. The parking lots shall be designed to facilitate safe pedestrian movement throughout the lot. This may include sidewalks internal to the site, cross walks, etc. The Zoning Administrator or Planning Commission may require pedestrian facilities where pedestrians and vehicular conflicts exist.
5. Shared parking is strongly encouraged where appropriate, particularly where abutting land uses have differing hours of peak usage. Cross Easements may be required to allow shared parking. Site plans that are proposing multiple land uses shall submit a shared parking study to justify the shared parking.
6. The development of smaller commercial buildings on out-parcels within large existing parking areas is encouraged as a means to break up the scale.

### **3.4 SERVICE AREAS**

Service areas shall be integrated into the overall site plan. The location shall address the needs of the facility while minimizing traffic or visual conflicts, noise or odors.

1. All facilities for service, including waste collection and storage facilities, loading and unloading areas, loading docks, storage facilities, dumpsters, recycling areas, fueling areas and vehicle service and maintenance areas shall be at the side or rear of the principal building.
2. Locations that face public roadways or abutting residential properties shall be avoided.
3. Overhead doors or other vehicle entrances and exits shall not be located on any façade that faces a public street or residential neighborhood.
4. Service areas shall be sized to fit the specific need of the buildings intended use. The smallest size to meet the building's future needs is encouraged.

5. Service areas shall be screened to minimize visibility from public and private streets, main entrances, abutting neighborhoods, public open spaces and walkways. Service areas shall be screened with architectural elements, such as walls or fences.
6. Wall and fence screening of service areas shall be at least 6 feet in height. The screening material shall be consistent with the building materials of the building.
7. Service areas shall be sited to accommodate the turning movement of service vehicles.
8. Where areas adjoining residential zones are likely to be used for truck loading, storage or driveways, the transitional zone must provide protection through use a solid masonry material or a combination of fencing and landscaping.
9. To reinforce the sense of natural surroundings and a consistent streetscape, auto service functions such as areas to store cars while they are being repaired, auto or truck work areas, truck loading docks, and garage door areas in commercial or retail areas shall be screened from public view.

### **3.5 OUTDOOR STORAGE**

1. Areas for any potential outdoor storage and sales areas shall be included in the initial site plan design. These areas shall be designed to complement the overall development.
2. Outdoor storage or temporary storage shall not be located in areas designated for parking.
3. Storage areas shall not be located between the building façade and street.
4. Storage areas shall be screened from public and adjoining property view. Screening shall include both fencing and landscaping.
5. Outdoor sales areas shall be identified on the site plan.

### **3.6 ROOF MOUNTED EQUIPMENT, DUMPSTER ENCLOSURES, AND GROUND MOUNTED EQUIPMENT**

1. Garbage collection areas shall be enclosed by opaque materials on all four sides, with doors to remove containers.

2. The screening of dumpsters enclosures shall ensure that the dumpster is not visible over the top of the enclosure. Where topography may expose interiors of garbage collection areas to view, Planning Commission may require screening to be correspondingly taller.
3. Landscape plantings are required around three sides of all dumpster screens to soften the effect of fencing. The landscaped area must be a minimum of five (5) feet wide.
4. The dumpster enclosure screening material shall be consistent with the building materials of the building.
5. Utility propane tanks, gas meters, electric meters, utility meters, backflow devices, and ground-mounted air conditioning or mechanical units shall be hidden from public view by screening. This shall not apply to propane tanks for sale at commercial locations.
6. Mechanical and roof mounted equipment shall be screened from public view and shall be compatible in color and material with the overall building material palette.
7. Planning Commission may relax screening requirements when areas are located so they are not visible from public streets or adjacent properties.

### **3.7 DRIVE-THROUGH FACILITIES**

Drive-through facilities, as permitted in the Zoning Ordinance, shall be subject to the following standards:

1. Where no street separates the use and a residential zoned property, at least 40 feet of separation shall be maintained between the residential lot line and the drive-through facility.
2. The location of drive-through windows and associated facilities (for example: communications systems and access aisles) shall be identified on all site plans.
3. The building shall front the primary street, not the drive-through aisles. The building shall be sited to maximize the distance for vehicle queuing while screening the drive-through operations (locate along the side or rear area of the structure).



4. Drive-through windows and associated facilities (for example: communications systems and menu board) shall not be permitted on the front façade of a structure. When a parcel fronts two streets the drive-through shall face the street of lower hierarchy.
5. Drive-through aisle between the right-of-way and a building shall require a ten (10) foot landscaped area, that includes street trees and shrubs.
6. Drive-through facilities shall include a trash can at the end of the aisle.
7. Vehicle stacking areas shall be provided in accordance with the Table 1: Required Stacking Spaces for Drive Throughs.

*Table 1: Required Stacking Spaces for Drive Throughs*

| Activity Type              | Minimum Stacking Spaces  | Measured From          |
|----------------------------|--|------------------------|
| Automated teller machine   | 3  | Teller                 |
| Bank teller lane           | 4  | Teller or window       |
| Car wash bay, full-service | 6  | Bay                    |
| Car wash bay, self-service | 3  | Bay                    |
| Dry cleaning/laundry       | 3  | Cleaner/Laundry Window |
| Gasoline pump island       | 2  | Pump Island            |
| Gatehouse, staffed         | 4  | Gatehouse              |
| Gate, unstaffed            | 3  | Gate                   |
| Pharmacy pickup            | 3  | Pharmacy Window        |
| Restaurant                 | 8  | Pickup Window          |
| Valet parking              | 3  | Valet Stand            |
| Other                      | Determined by Zoning Administrator in consideration of an approved study prepared by a registered engineer with expertise in Transportation Engineering. |                        |

### **3.8 OUTDOOR SEATING AND DINING AREAS**

1. Outdoor dining areas shall be separated from parking areas and drive aisles by a five (5) foot wide landscaped area, a fence, or planters.

2. Outdoor dining areas that are separated from parking areas or drive aisles by a landscaped area shall include a mixture of shrubs and/or trees.
3. If an outdoor dining area is separated from parking area or drive aisles by a fence, the fence shall be a minimum of twenty-four (24) inches tall or a maximum of thirty-six (36) inches tall. The fence shall be made of iron, aluminum, or comparable material. Vinyl and wood are not acceptable materials.
4. Outdoor dining areas that are separated from parking areas or drive aisles by planters, the planter shall be made of masonry and be spaced no more than thirty-six (36) inches apart unless providing ADA access. The planter shall be at a minimum of twenty-four (24) inches tall.
5. Where tables are placed next to a building's exterior wall, a clear area of at least four (4) feet in width shall be maintained to building entries.
6. Property clearance around fire and emergency facilities (e.g., fire hydrants, exits, etc.) shall be maintained at all times.

### **3.9 FENCES, WALLS, AND HEDGES**

1. Fences, walls, and hedges must comply with Section 3.10, Sight Distance Requirements.
2. No fence or wall shall be installed so as to block or divert a natural drainage flow on to or off of any other land.
3. No fence or wall shall be installed over an occupied public utility and drainage easement.
4. Appearance – Fences and walls shall comply with the following standards:
  - a. Customary Materials – Fences and walls shall be constructed of materials customarily used and manufactured as common fence or wall materials, including solid wood, brick, masonry, stone, chain link, wrought iron, decorative metal materials, or products designed to resemble these materials. Chain link fencing approved as part of a Site Plan shall be vinyl coated and colored dark green, brown or black. Chain link fencing shall not be used as part of a landscaped buffer.
  - b. Prohibited Materials – Fences and walls constructed of debris, junk, rolled plastic, sheet metal, plywood, or other waste materials are prohibited in all zoning districts, unless such materials have been recycled and reprocessed into building

materials marketed to the general public and designed for use as fencing or wall materials. Vinyl slats or thin fabric inserts or coverings are not allowed to be used in any fencing.

- c. Finished Side to Outside – All fences and walls shall be oriented with the “good” or “finished” side facing outward (i.e. one side has visible support framing and the other does not) rather than facing the interior of the lot. This provision shall not preclude the placement of a shadow-box type fence.
  - d. Uniformity of Materials – Fencing and wall segments located along a single lot side shall be of uniform height, material, type, color, and design and shall be uniform for the entire length of the fence or wall, except where a fence or wall segment transitions from one yard to another or from one height to another.
5. Maintenance Required – All fences and walls shall be maintained in good repair and in a safe and attractive condition, including but not limited to replacement of missing, decayed, or broken structural and decorative elements.

### **3.10 SIGHT DISTANCE REQUIREMENTS**

- 1. At any public or private street intersection and at the access point for private driveways to public or private streets, a clear zone for sight distance shall be maintained. No landscape material, signage, fences, or other apertures that exceeds the height of two and one half (2.5) feet or lower than six (6) feet shall be in any sight distance clear zone as indicated in Figure 1: Visibility at Intersections
- 2. In order to protect safe sight visibility lines for street intersections, landscape buffers and shrubs shall not be located within thirty-five (35) feet of a street corner.

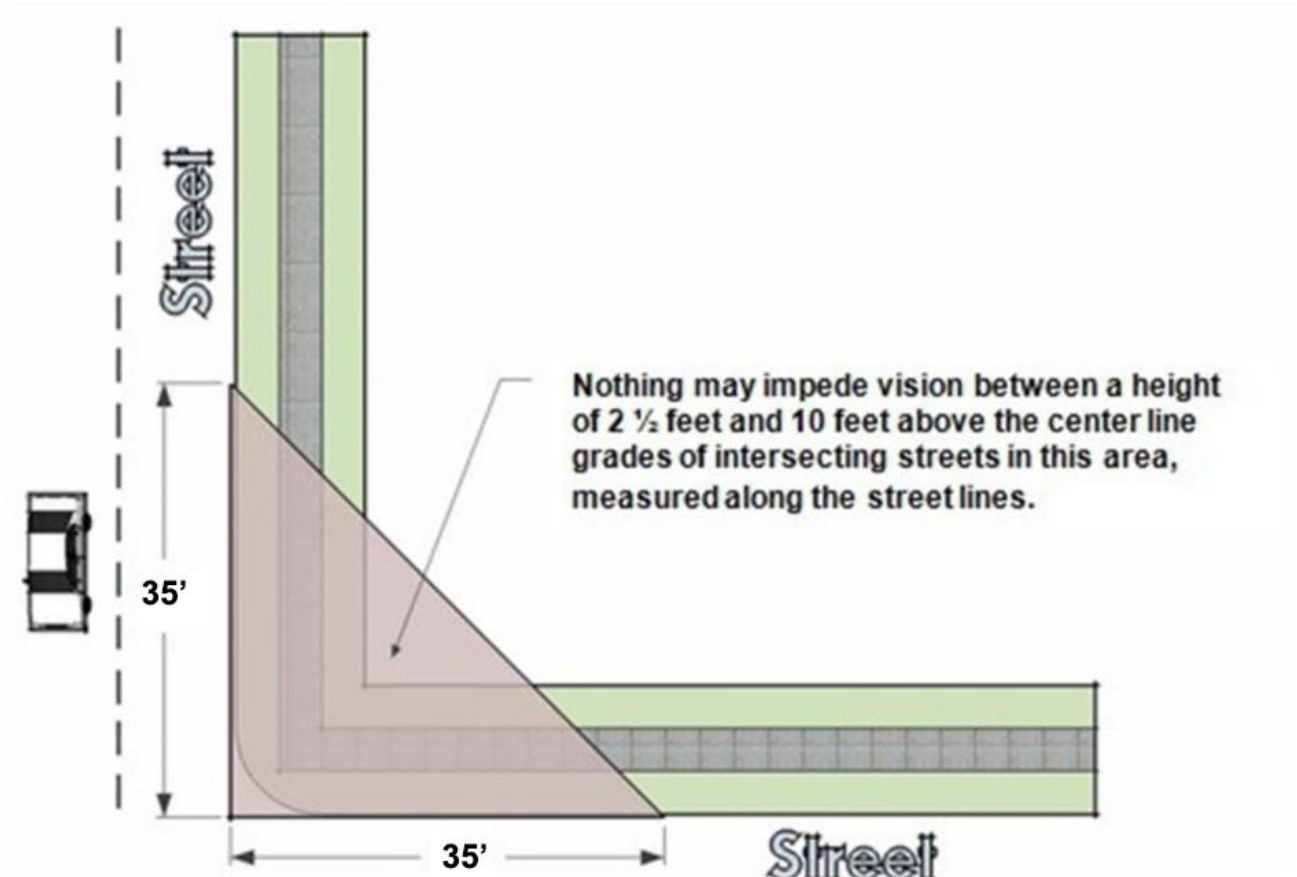
### **3.11 STORMWATER MANAGEMENT**

In an effort to minimize the effects of runoff from storm events, any development project specified in these Standards shall be required to comply with all applicable stormwater management requirements, as specified in Stormwater Ordinance 16-02 and the City of Portland Minimum Drainage Requirements.

**3.12 SPECIAL FLOOD HAZARD AREAS**

In an effort to minimize the effects of flood hazard events in areas identified in special flood hazard areas, any development project specified in these Standards shall be required to comply with all requirements for reduction of flood hazards as specified in Article IX: Floodplain Districts, of the Combined Zoning Ordinance of the City of Portland, Tennessee and the Portland Planning Region.

*Figure 1: Visibility at Intersections*



## **CHAPTER 4 ARCHITECTURE**

---

Building design shall be influenced by architectural features compatible with the characteristics of the surrounding area, and shall reinforce a human scaled environment through careful consideration of architectural form, massing, detail, material and color. These design standards establish criteria, but are not intended to dictate building styles.

In order to ensure compliance with this section of the requirements, scaled architectural drawings of all building elevations and other structures shall be submitted for review and approval by the Planning Commission. The drawings shall be of sufficient detail to explain the type of proposed materials.

This Chapter (Site Design Standards) shall apply to multi-family, mixed use, community facility, commercial, and industrial site plans. These standards shall not apply single-family detached or two-family dwelling detached units, unless three or more dwelling units are built on a single lot.

### **4.1 INTENT**

1. Development that provides a positive experience for the motorist driving by and the pedestrian viewing building up close, and;
2. Enhances Portland's rural character, and;
3. Exhibits a thoughtful consideration of scale, form, orientation, height, setback, massing, materials and architectural features, and;
4. Strives to restore and/or reuse older buildings to maintain the character of Portland, replacement/preservation of historic buildings is strongly encouraged, and;
5. The facades of buildings visible from a residential property shall use forms, materials, and details which are residential in nature and appearance. Service areas, parking lots, outdoor storage and other similar features shall not be visible from residential properties.

## **4.2 GENERAL ARCHITECTURE DESIGN STANDARDS**

1. New buildings shall be designed to fit the individual character of the site and the intended use and users of the building. Designs should be selected to reflect the Portland context, by careful siting, use of compatible materials, and landscaping of the site so that it blends with its surroundings.
2. Buildings and site elements shall be designed to human scale. The forms, massing and openings of buildings shall be proportional.
3. The design of freestanding structures (such as ATM's, garages, canopies, storage units, recycling or trash enclosures, cart corrals, and the like) shall coordinate with the primary building through the repetition of form, materials, details, and color.

## **4.3 FAÇADE DESIGN**

All Buildings shall have an aesthetically appropriate and human scaled façade to the street, internal drives, parking areas, and surrounding neighborhoods. Entrances shall be easy to distinguish and reinforced through site and architectural features wherever possible, clearly visible from the street.

The facades of industrial buildings shall meet the façade elements, offsets, or other requirements of this section at the office entry or other public entry. If any industrial site has more than one structure, the structure along the primary street shall meet the requirements of this section, as required above.



### **4.3.1 Façade Elements**

As the primary and/or front façade, it shall be designed in a manner to clearly distinguish it from the other facades and to define the customer entry. This façade shall contain 2 or more of the following elements, See Table 2: Example of Facade Elements:

1. Canopy
2. Covered porch or arcade
3. Varied roof heights
4. Pilasters
5. Display windows
6. Outdoor seating
7. Recesses or projections (in keeping with the scale of the building)
8. Peaked roof
9. Unique architectural details (in keeping with the scale of the building)

10. Other features designed to add scale and visual interest to the façade

Table 2: Example of Facade Elements

|                       |   |
|-----------------------|---|
| Canopy                |   |
| Cover porch or arcade |  |

Varied roof heights





Pilasters



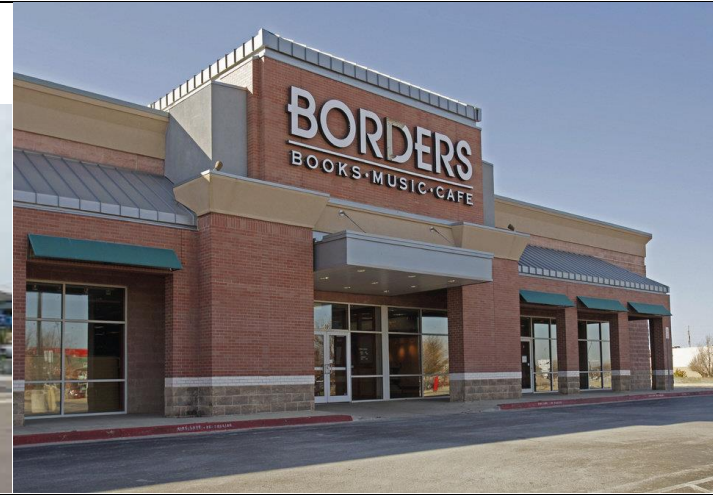
Display windows



Outdoor seating



Recesses or projections



Peaked roof

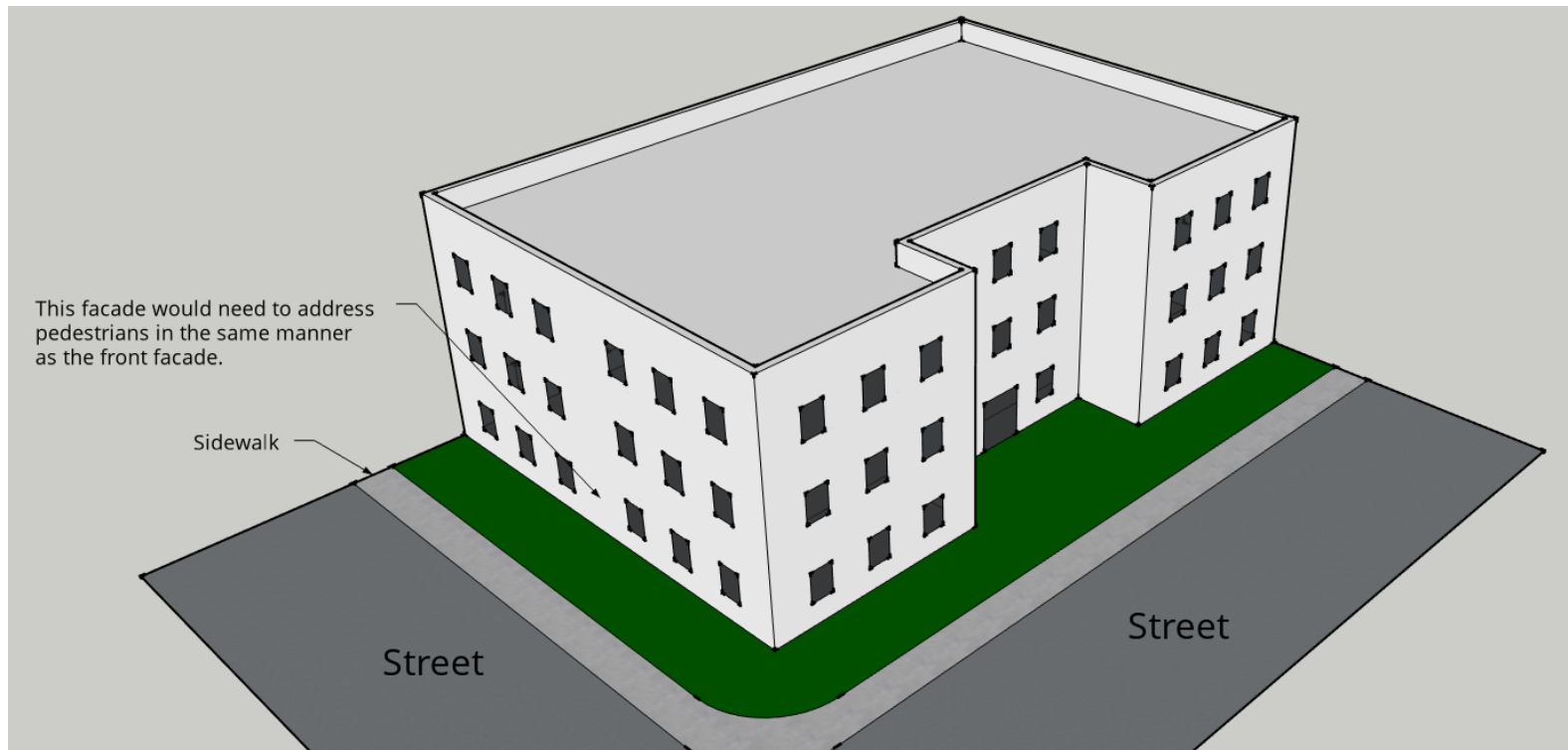




#### 4.3.2 Rear and Side Facades

1. All facades facing public roads, residential neighborhoods or abutting properties shall be designed to match or compliment the main entrance façade. Blank facades are prohibited.
2. For the rear of the building, it is recommended that if the rear of the building is to have pedestrian access (especially where the building backs up to residential areas), the rear of the building should match the front of the building in both elements and materials.
3. Any building with a corner or side exposed (along side streets) to pedestrians should be treated with the same design considerations and materials as the front of the building. See Figure 2: Side Facades on a Corner Lot.

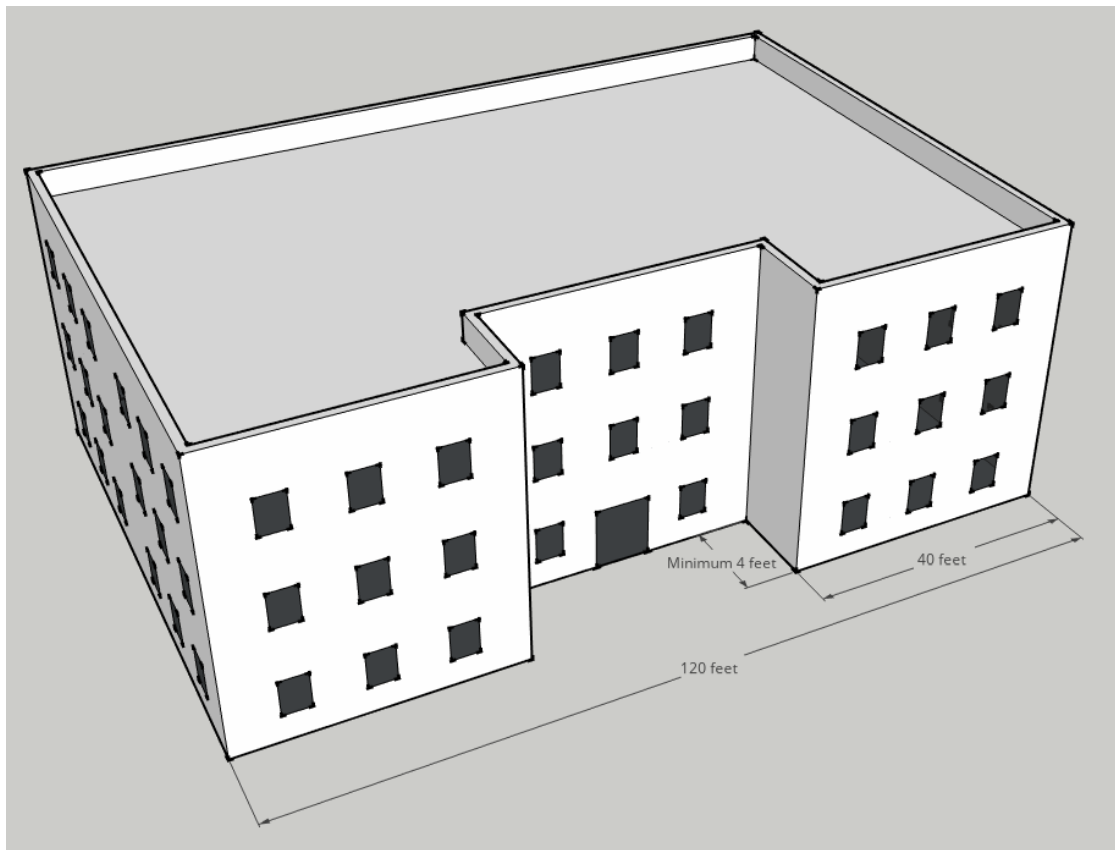
*Figure 2: Side Facades on a Corner Lot*



#### 4.3.3 Offsets

The maximum length of the plane of any façade is 40'; exterior walls of any building longer than 40' shall have recesses or projections at a minimum depth of 10% of the longer adjacent unbroken wall length and be proportional to the building's height and length. Projections used to break up the length of the building shall extend to the ground. The Design Review Committee may wave this requirement for industrial uses. See Figure 3: Building Offset Diagram.

*Figure 3: Building Offset Diagram*



#### **4.3.4 Other Façade Elements**

1. All exterior components, such as signs, lighting, landscaping, and other elements shall be in scale with, and complimentary to the main entrance façade.
2. Window and door openings shall have a vertical orientation and shall be vertically aligned between floors.
3. Where a clearly established development character and scale exists, new infill development shall include:
  - a. window and door openings with area ratios and proportions similar to those on adjoining buildings,
  - b. key design elements of surrounding buildings with respect to windows, door, rhythm of bays, detailing, roof forms, materials and colors.
4. All vents, downspouts, flashing, electrical equipment, meters, HVAC Equipment, service areas, loading docks, service connections and other functional elements shall be treated as an integral part of the architecture and incorporated into the façade design. Meters, Utility connections shall be contained in service closets, behind walls, or located out of the view of the general public.

#### **4.4 BUILDING MATERIALS**

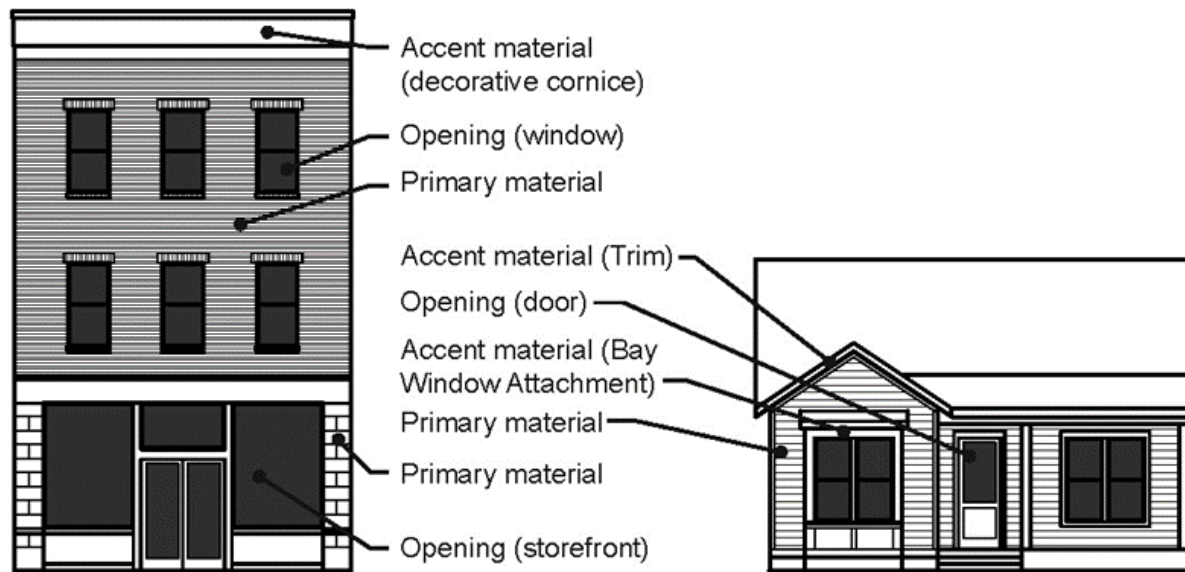
Buildings materials shall be treated as a significant design element in defining the appearance of the building.

1. Materials for building facades shall be selected from Table 3: List of Permitted Building Materials by Zoning and/or Use according to the requirements relative to zoning or use designation in the table.
2. The building materials on a multi-family and townhome structure shall be similar to that of the surrounding properties. i.e. If a multi-family or townhome site is abutting single-family residential with siding, fiber cement or composite siding would be appropriate. If the single-family residential is brick, brick would be appropriate for the multi-family.
3. Building facades shall be built of minimum of two primary materials but no more than three primary materials on any one façade (See Figure 4: Primary and Secondary Materials Diagram).

Table 3: List of Permitted Building Materials by Zoning and/or Use

|                                       | Multi-Family & Townhomes | CBD | CMU, NMU, GCS, HCD, ISD, and Commercial Uses in Industrial Districts | RMU, NSD & OPS | Industrial |
|---------------------------------------|--------------------------|-----|--|----------------|------------|
| <b>Masonry</b>                        |                          |     |  |                |            |
| Brick                                 | ✓                        | ✓   | ✓  | ✓              | ✓          |
| Stone                                 | ✓                        | ×   | ✓  | ✓              | ✓          |
| Split-faced/fluted concrete block     | ×                        | ×   | ×  | ×              | ✓          |
| Concrete block                        | ×                        | ×   | ×  | ×              | ✓          |
| Cast Stone                            | ✓                        | ×   | ✓  | ✓              | ✓          |
| Manufactured stone veneer siding      | ✓                        | ×   | ✓  | ✓              | ✓          |
| Stucco (authentic)                    | ✓                        | ×   | ✓  | ✓              | ✓          |
| <b>Metal</b>                          |                          |     |  |                |            |
| Architectural Metal Panels            | ×                        | ×   | ✓  | ×              | ✓          |
| Metal Siding (ribbed & standing seam) | ×                        | ×   | ×  | ×              | ×          |
| <b>Wood</b>                           |                          |     |  |                |            |
| Wood siding/shingles                  | ✓                        | ×   | ✓  | ×              | ✓          |
| <b>Other Materials</b>                |                          |     |  |                |            |
| Concrete, finished                    | ×                        | ×   | ×  | ×              | ✓          |
| Concrete, unfinished                  | ×                        | ×   | ×  | ×              | ✓          |
| Fiber cement siding, shingles, trim   | ✓                        | ×   | ✓  | ✓              | ✓          |
| Fiber cement panels                   | ×                        | ×   | ×  | ×              | ✓          |
| Composite siding                      | ✓                        | ×   | ✓  | ✓              | ✓          |
| Composite panels                      | ×                        | ×   | ×  | ×              | ✓          |
| Vinyl siding                          | ×                        | ×   | ×  | ×              | ×          |
| Vinyl panels                          | ×                        | ×   | ×  | ×              | ×          |
| EFIS (upper stories only)             | ✓                        | ×   | ✓  | ✓              | ✓          |
| ✓ = Permitted<br>× = Not Permitted    |                          |     |  |                |            |

Figure 4: Primary and Secondary Materials Diagram

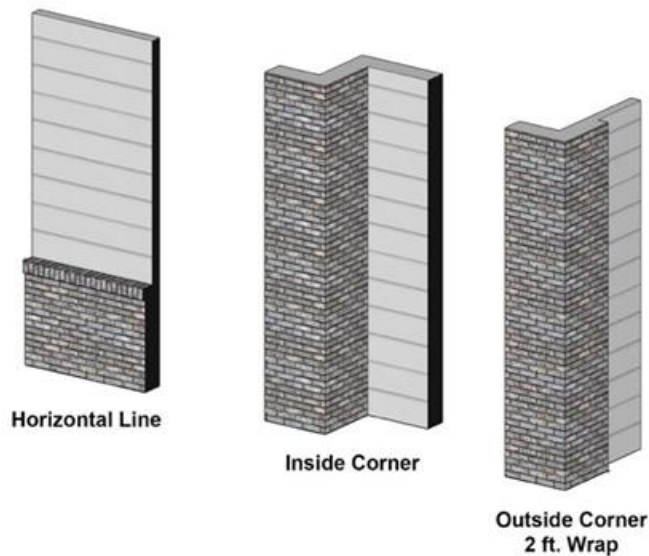


4. Primary façade materials shall not change at outside corners. Façade materials only change along a horizontal line, at an inside corner, or outside corners where the materials wrap the corner a minimum of two (2) feet (See Figure 5: Material Change Diagram).
5. The Multi-Family & Townhome building materials standards shall apply to Community Facilities Uses in residentially zoned areas. Community Facility uses in a Commercial Zoning District shall meet the building material standards for that zoning district. Community Facility uses in an Industrial Zoning District shall meet the Commercial Uses in an Industrial District standard.



6. Materials not listed may be approved by Planning Commission. It shall be the responsibility of the Developer to prove that proposed materials are as durable as the allowed materials.
7. Material samples may be required at the discretion of the Planning Commission and/or Zoning Administrator.

*Figure 5: Material Change Diagram*



## 4.5 ROOFLINES

Roof forms shall be appropriate to a building's design and scale. Roof lines should reduce the mass of large buildings and emphasize entrances and where possible provide shelter and shade for the pedestrian.

1. Flat roofs or low-pitched roofs with parapet walls are encouraged for larger commercial buildings.
2. A particular roof form shall be applied to the entire roof, rather than terminating at less visible points, such as the building's rear. Alternative roof forms may be used if appropriate for a particular acceptable architectural style.

3. Roofs that are visible from the road shall be finished with colors and features consistent with the architecture of the façade.
4. Mechanical and roof mounted equipment shall be screened from public view and shall be compatible in color and material with the overall building material palette.

#### **4.6 AWNING AND CANOPIES**

Awnings and canopies can enhance the appearance and function of a building by providing shade, defining the entrance, shelter and shadow patterns and visual interest.

1. If used, all awnings and canopies shall be integrated as part of the design and located directly over doors and windows.
2. Awnings and canopies shall not be made of reflective material such as metal or plastic. Their color and style shall be complimentary to the façade of the building.

#### **4.7 ALTERATIONS AND ADDITIONS**

All alterations and additions to existing structures shall use the opportunity to add visual interest to the building and to enhance the original structure.

1. Proposed alterations must be designed to complement the existing building. Where the existing building does not meet the design standards, the owner/developer is strongly encouraged to upgrade the building. Plans submitted for approval must show the proposed improvements along with the existing building.
2. Additions or alterations shall complement or match the materials, form color and detailing of the original structure.

## **CHAPTER 5 LIGHTING**

---

This Chapter (Site Design Standards) shall apply to multi-family, mixed use, community facility, commercial, and industrial site plans. These standards shall not apply single-family detached or two-family dwelling detached units, unless three or more dwelling units are built on a single lot.

### **5.1 INTENT**

Lighting for commercial properties shall be designed to provide the minimum level of illumination necessary for security, safety and visual appeal for the pedestrian and motorists.

### **5.2 LIGHTING DESIGN STANDARDS**

1. A Lighting plan shall be submitted as part of the site plan review.
2. To reduce adverse impacts on adjacent sites and minimize energy consumption, lighting shall be carefully located and its intensity shall be the minimum necessary for safety.
3. Lighting levels shall be as even as possible.
4. Light fixtures, which cast light primarily downward, shall be used.
5. Does not cause distractions or hazards to motorists and pedestrians, minimizes skyglow
6. Warm lighting colors are required; blue-white color is prohibited
7. Any lighting used to illuminate off-street parking areas shall be directed away from property in any residential district in such a way as not to create a nuisance.
8. Lighting shall not exceed one-half (1/2) foot candle at any property line.

9. Parking areas and pedestrian ways on commercial and industrial zoned properties shall be lighted to an intensity of at least 0.5-foot candle. No such lighting shall be directed in a manner which illuminates adjoining residential properties or residential zoned properties.
10. Where lighted parking areas are located adjacent to residential zones, lighting shall be designed to minimize illumination across the boundary.

## **CHAPTER 6 PROCEDURE, APPEALS, AND PENALTIES**

---

### **6.1 PROCEDURE**

The Zoning Administrator shall prepare checklist and applications, as needed, to require submittal documents that ensure compliance with these standards and other applicable City ordinances. Said checklist and applications may be combined with other checklist and applications where they logically fit (i.e. if Planning Commission is acting as Design Review Commission, they shall be in the Site Plan Checklist and Application).

### **6.2 DESIGN STANDARD REVIEWS**

The Zoning Administrator shall review all projects for compliance with these design standards and make a recommendation to the Planning Commission, as required by these standards.

1. Review of the design standards may occur concurrent with site plan approval by Planning Commission or City Staff.
2. Site Plans eligible for minor site development plan review shall be subject to these design standards and may be approved by the Zoning Administrator as part of a minor site development plan review.

### **6.3 APPEALS**

1. To appeal a decision, the developer and/or their representative shall provide a narrative to the City Planning Staff explaining the specific section of the Design Guidelines being appealed and why that section is placing a unique hardship on the proposed project. The developer and/or representative shall also provide the appropriate number of copies of the site plan and/or architectural elevations for review, as required by the Zoning Administrator.
2. Appeals of Staff design standard reviews may be made to the Design Review Committee.
3. Appeals of Design Review Committee's design standards reviews may be made to the Planning Commission, unless the Planning Commission is acting as the Design Review Committee. When the Planning Commission is acting as the Design Review Committee the Board of Mayor and Aldermen shall hear any appeals.

## **6.4 PENALTIES**

Sites shall be constructed in accordance with the approved design elements and site plan. Owners and/or Developers seeking a Certificate of Use and Occupancy shall have complied with the approved design elements and site plan prior to receiving a permanent Certificate of Use and Occupancy permit. If a site does not comply with the approved design elements and site plan the Design Review Committee and Planning Commission will have to approve any changes prior to issuance of Certificate of Use and Occupancy. Temporary Certificate of Use and Occupancy may be issued as allowed by the Zoning Ordinance when a site does not comply with the design elements and site plan. Temporary Certificate of Use and Occupancy may only be issued while changes are being made to bring the site into compliance with the approved design elements and site plan.

The Design Review Standards shall be enforced by the Zoning Administrator, who shall have the right to enter upon any premises necessary to carry out his duties in this enforcement. Any person violating any provision of this ordinance shall be guilty of a misdemeanor, and upon conviction shall be fined fifty dollars (\$50.00) for each offense. Each day such violation shall continue shall constitute a separate offense.